

**PLANNING COMMISSION AGENDA
REGULAR MEETING
MONDAY, SEPTEMBER 17, 2012**

**STUDY SESSION 4:30 P.M. CITY COUNCIL CHAMBERS
MEETING 5:00 P.M. CITY COUNCIL CHAMBERS**

1. Meeting Called to Order
2. Roll Call
3. Determination as to Legality, Notice of Meeting and Agenda
4. Reading of the Minutes of the August 20, 2012, Regular Meeting.
5. Public comment period. The general public is invited to address the Planning Commission regarding any item on this agenda. The overall and individual speaking time allotments may be limited by the Chair.
6. Consider request of CLOU, LLC c/o Scott Tallman to erect accessory building that is not architecturally harmonious with the principal structure at 115 South Ridge Road, Fremont, Dodge County, Nebraska. Article 7, Section 704. RL – Lake and River District.
7. Consider request of Connie Stiver to combine the East 60' of Lot 1, Block 6, the West 6' of Lot 1, Block 6 and Lot 2, Block 6, R. Kittles Addition, Fremont, Dodge County, Nebraska into one lot. R1 – Single Family Residential District.
8. Consider the Preliminary Plat of Fremont Commons, a tract of land located in the Northwest Quarter of Section 17, Township 17 North, Range 9 East of the 6th P.M., Dodge County, Nebraska.
9. Consider request of Fremont Commons, LLC to rezone a tract located in the Northwest Quarter (NW ¼) of Section 17, Township 17 North, Range 9 East from AG – Agricultural/Urban Reserve District to GC – General Commercial District and R-3 Mixed Density Residential District with TND – Traditional Neighborhood Development District Overlay.
10. Consider the Final Plat of Deer Pointe Second Addition, consisting of Lot 6, Block 2 Deer Pointe Addition and part of the S ½ of the NW ¼ of Section 18, Township 17 North, Range 9 East, Dodge County, Nebraska.
11. Study Session – Board of Adjustment & Findings of Fact

THIS MEETING WAS PRECEDED BY PUBLICIZED NOTICE IN THE FREMONT TRIBUNE AND THE AGENDA DISPLAYED IN THE LOBBY OF THE MUNICIPAL BUILDING AND POSTED ONLINE AT WWW.FREMONTNE.GOV; AND DISTRIBUTED TO THE PLANNING COMMISSION, MAYOR AND CITY COUNCIL ON SEPTEMBER 12, 2012 AND IS OPEN TO THE PUBLIC. A COPY OF THE AGENDA WAS KEPT CONTINUALLY CURRENT AND AVAILABLE TO THE PUBLIC AT THE OFFICE OF THE CITY PLANNER, 400 EAST MILITARY. A COPY OF THE OPEN MEETING LAW IS POSTED CONTINUALLY FOR PUBLIC INSPECTION LOCATED NEAR THE ENTRANCE DOOR BY THE AGENDAS. THE PLANNING COMMISSION RESERVES THE RIGHT TO ADJUST THE ORDER OF ITEMS ON THIS AGENDA.

STAFF REPORT

TO: Planning Commission

FROM: Rian Harkins, Planning Director

DATE: 10 Sept 2012

SUBJECT: Request of CLOU, LLC (c/o Scott Tallman) for approval of a Conditional Use Permit to construct an accessory building that is not architecturally harmonious with the principal structure, Fremont, Dodge County, Nebraska (115 South Ridge Road)

Recommendation: Staff recommends approval.

Request:

The applicant requests that the lot listed above be approved for a Conditional Use Permit for a non architecturally harmonious accessory building in the RL Lake and River Residential Zoning District.

Background:

The property is located in the southwest quadrant of the City's Extraterritorial Jurisdiction along South Ridge Road. The area proposed for the Conditional Use Permit abuts RL Lake and Residential Zoning on all four sides. The conditional use permit is required because the applicant desires to erect a Cleary building on the property near the primary residence.

Findings:

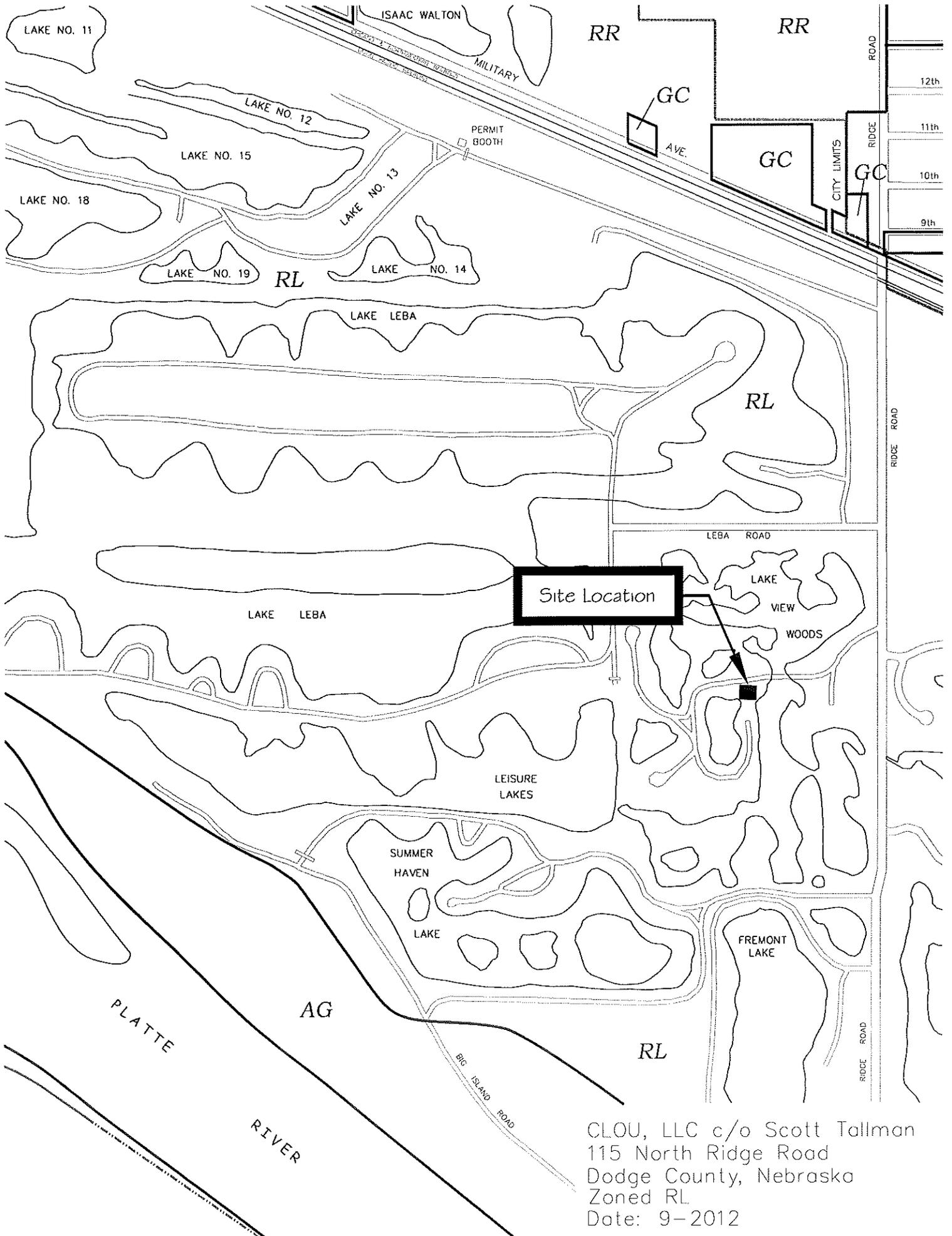
In accordance with Section 1203, Table 12-1, the following criteria shall utilized to determine approval of the Conditional Use Permit. The proposed use will comply with most of the criteria with the proposed conditions.

	CRITERIA	Compliance
Land Use Compatibility Development Density	Site area per unit or floor area ratio should be similar to surrounding uses if not separated by major natural or artificial features.	Yes
Height and Scale Height and Bulk	Development should minimize differences in height and building size from surrounding structures. Differences should be justified by urban design considerations.	Yes
Setbacks	Development should respect pre-existing setbacks in surrounding area. Variations should be justified by site or operating characteristics.	Yes

Building Coverage	Building coverage should be similar to that of surrounding development of possible. Higher coverage should be mitigated by landscaping or site amenities.	Yes
Site Development		
Frontage	Project frontage along a street should be similar to lot width.	Yes
Parking and Internal Circulation	Parking should serve all structures with minimal conflicts between pedestrians and vehicles.	Yes
	All structures must be accessible to public safety vehicles.	Yes
	Development must have access to adjacent public streets and ways. Internal circulation should minimize conflicts and congestion at public access points.	Yes
Landscaping	Landscaping should be integral to the development, providing street landscaping, breaks in uninterrupted paved areas, and buffering where required by surrounding land uses. Parts of site with sensitive environmental features or natural drainage ways should be preserved.	Yes
Building Design	Architectural design and building materials should be compatible with surrounding areas or highly visible locations	NA
Operating Characteristics		
Traffic Capacity	Project should not obstruct traffic on adjacent streets. Compensating improvements will be required to mitigate impact on street system operations.	Yes
External Traffic Effects	Project design should direct non-residential traffic away from residential areas.	Yes
Operating Hours	Projects with long operating hours must minimize effects on surrounding residential areas.	Yes
Outside Storage	Outside storage areas must be screened from surrounding streets and less intensive land uses.	Yes
Public Facilities		
Sanitary Waste Disposal	Developments within 500 feet of a public sanitary sewer must connect to sewer system. Individual disposal systems, if permitted, shall not adversely affect public health, safety, or welfare.	Yes
	Sanitary sewer must have adequate capacity to serve development.	Yes
Utilities	Project must be served by utilities.	Yes
	Rural estate subdivisions should be located in designated areas which can accommodate utility and infrastructure installation consistent with the need to protect the environment and public health.	NA
Comprehensive Plan	Projects should be consistent with the City of Fremont's Comprehensive Development Plan.	Yes

The proposed conditional use meets the criteria of Table 12-1 of the Zoning Ordinance as well as the general intent of the Zoning Ordinance. Additionally, the proposed conditional use is in general conformance with the Comprehensive Plan.

Staff recommends approval based the fact that the accessory building is well screened from the primary access point to the site, and is a significant distance from said access point.



CLOU, LLC c/o Scott Tallman
 115 North Ridge Road
 Dodge County, Nebraska
 Zoned RL
 Date: 9-2012

ERICKSON SULLIVAN ARCHITECTS
209 South 9th Street
Lincoln, NE 68508
TEL: 402.475.1177 FAX: 402.475.1100

CONSULTANTS:

SITE PLAN

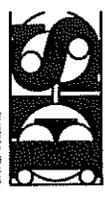
CLOU LLC

LANDVIEW RECORDS
DODGE COUNTY, NE

PROJECT NO. 130121

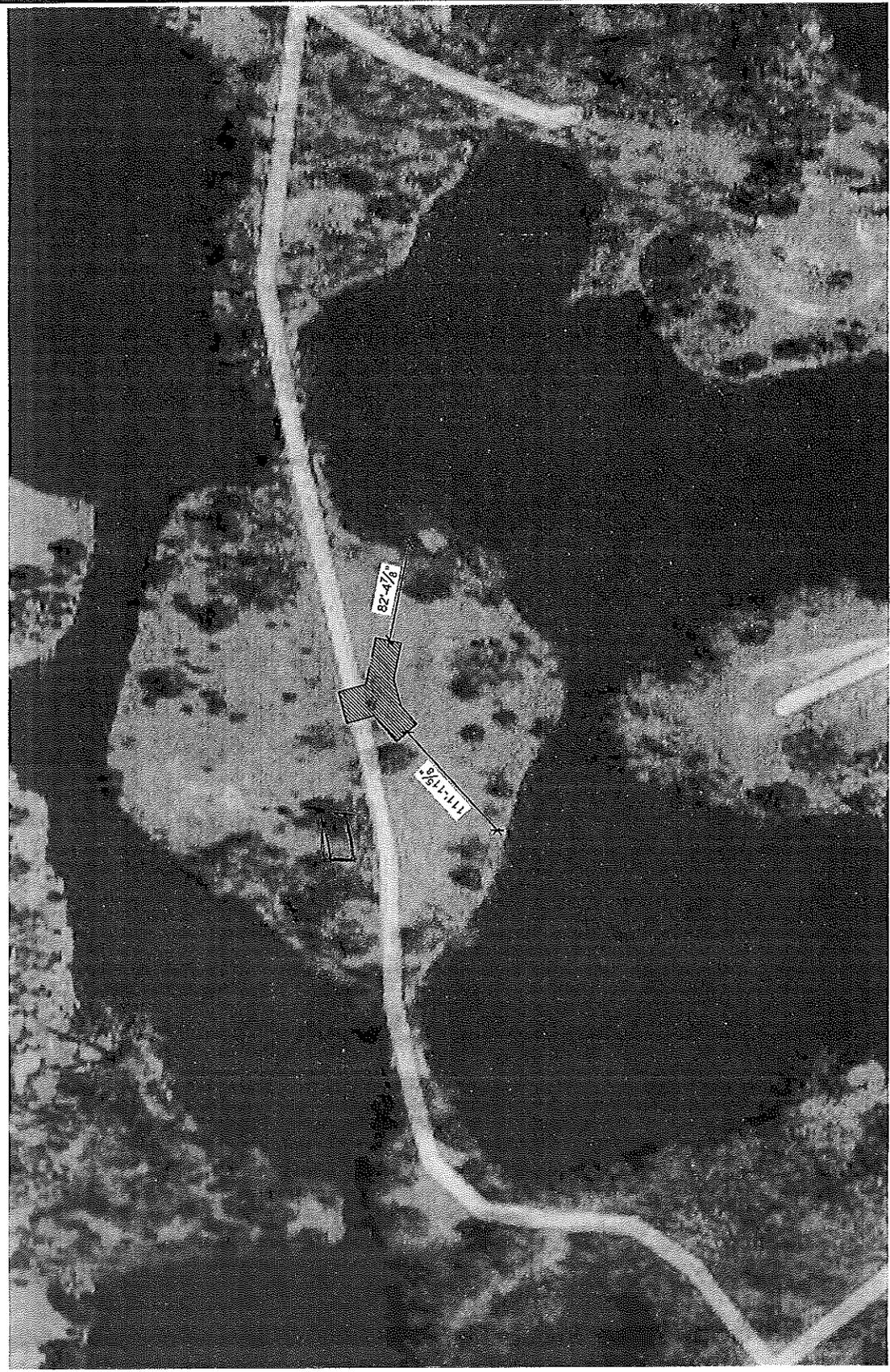
PERMIT SET

DATE: 06.23.12



**ERICKSON
SULLIVAN**
ARCHITECTS
209 South 9th Street
Lincoln, NE 68508
TEL: 402.475.1177 FAX: 402.475.1100

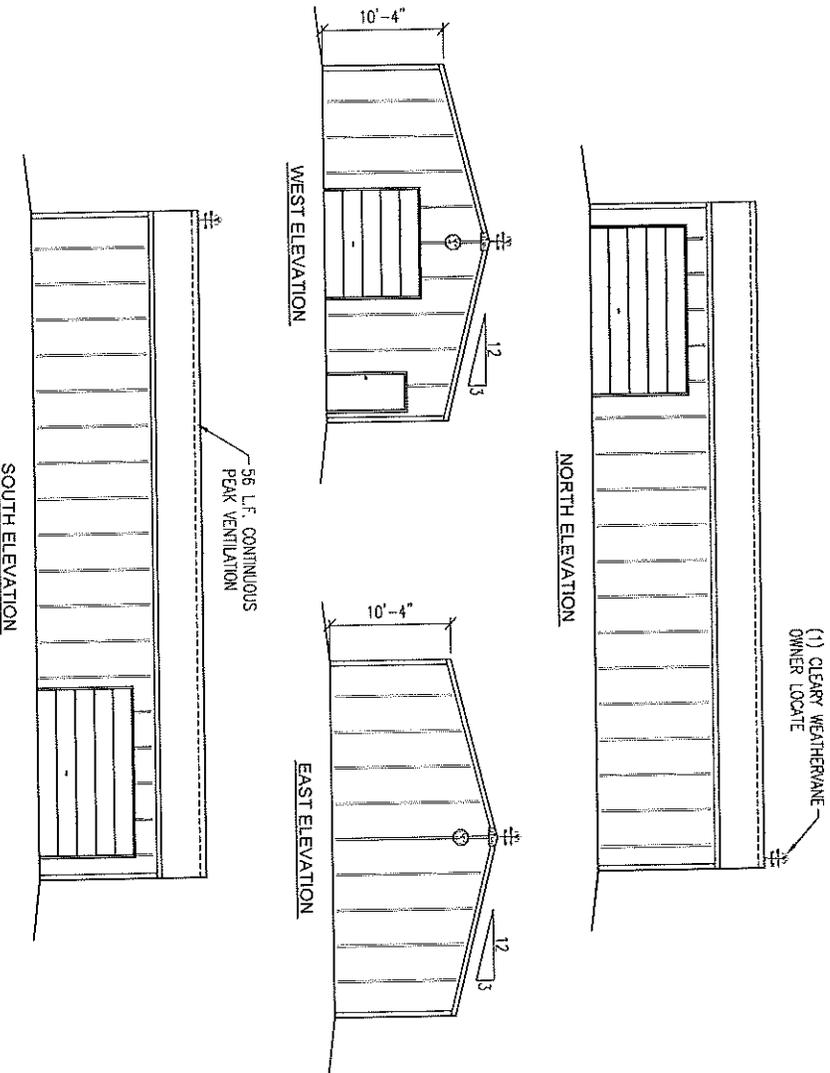
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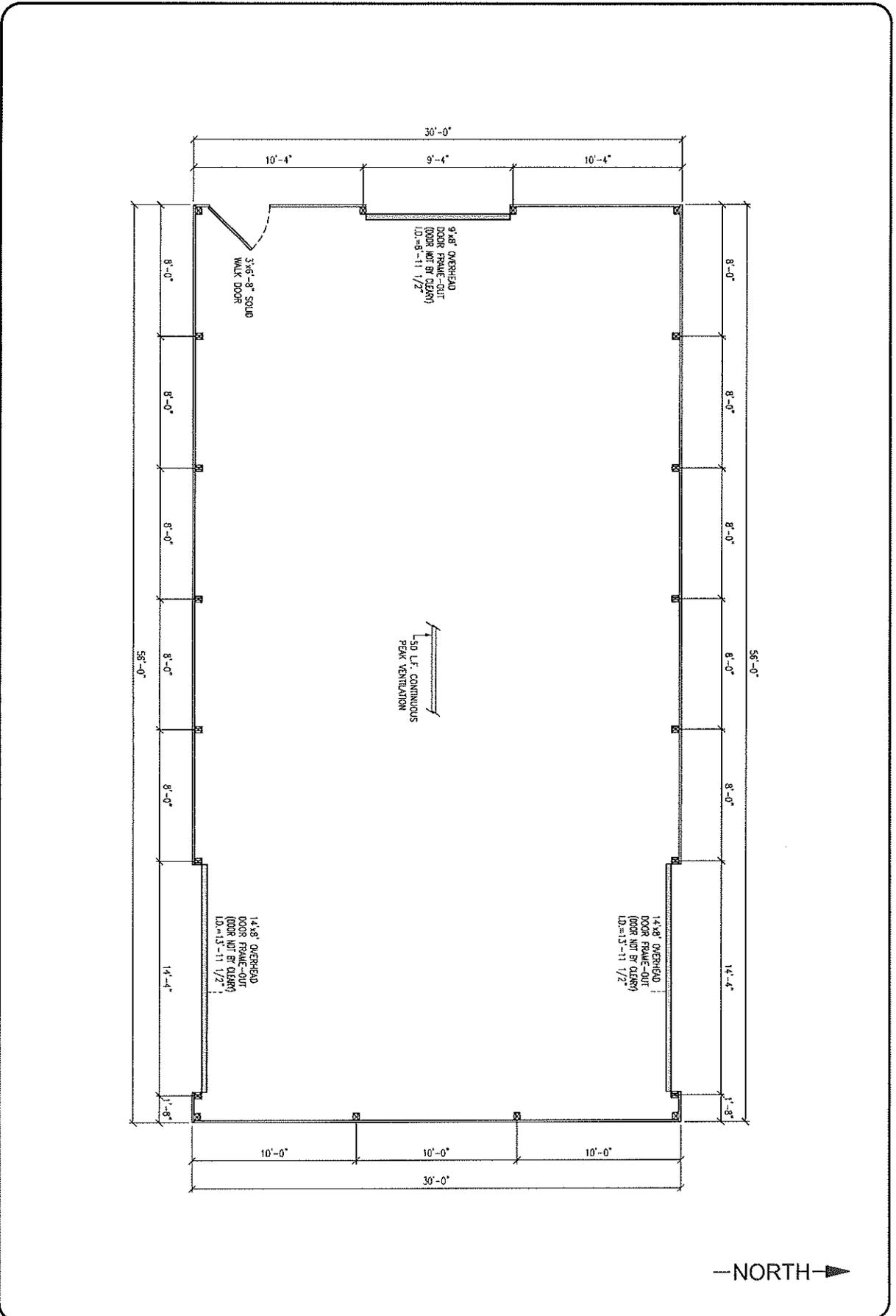




Proposed Building for:
Clou, LLC
30'x56'x10'-4" Commercial



We Protect What You Value



File Name and Path: I:\Comment\1\CAD FILES\2012\1\Proposals\PR2012186\PR2012186-FP.dwg
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<p>DATE: 8/29/2012 DRAWN BY: T. KALDENBERG SCALE: NONE SALES SPECIALIST: NELSON FREMONT</p>	<p>REVISIONS:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>---</td> <td>---</td> </tr> <tr> <td>2</td> <td>---</td> <td>---</td> </tr> <tr> <td>3</td> <td>---</td> <td>---</td> </tr> </tbody> </table>	NO.	DATE	BY	1	---	---	2	---	---	3	---	---	<p>CLOU, LLC 30'x56'x10'-4" COMMERCIAL FLOOR PLAN</p>	<p>TEARY BUILDING CORP. 190 PAQU STREET WILMINGTON, DE 19804 (800) 373-5550 CharneyBuilding.com</p>
NO.	DATE	BY													
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<p>JOB NO. PR2012186</p>		<p>SHEET 2 OF 2</p>													

STAFF REPORT

TO: Planning Commission

FROM: Rian Harkins, Planning Director

DATE: 10 September 2012

SUBJECT: Lot Combination request to combine the East 60' of Lot 1, Block 6, and the West 6' of Lot 1, Block 6 and Lot 2, Block 6, R. Kittles Addition Fremont, Dodge County, Nebraska (735 E 2nd Street) (Stivers)

Recommendation: Staff recommends approval.

Request:

The applicant seeks approval of a lot combination at the address listed above in order to maximize their buildable space..

Background:

The property is located along 2nd Street, and is currently zoned R-1 Single Family Residential. All of the adjacent properties are zoned R-1 Single Family Residential and are neighborhood conservation in nature according to the comprehensive plan.

Findings:

The proposed lot split will continue to allow the property to meet the intent of the R-1 Single Family Residential zoning district, and is in conformance with the proposed Comprehensive Plan, Blueprint for Tomorrow.

SECOND STREET

72.00'

60.00'

PROPOSED
LOT LINE

Lot 2
Block 6

Lot 1
Block 6

EXISTING
LOT LINE

140.00'

140.00'

MAPLE STREET

132.00'

COMBINE INTO TWO LOT



Connie Stiver
735 Second Street
Fremont, Nebraska
Zoned R1
Date: 9-2012

735

129

125

STAFF REPORT

TO: Planning Commission

FROM: Rian Harkins, Planning Director

DATE: 10 September 2012

SUBJECT: Preliminary Plat for Fremont Commons, located in the NW ¼ of Section 17, Township 17 North, Range 9 east of the 6th P.M., Dodge County, Nebraska (Fremont Commons, LLC)

<p>Recommendation: Staff recommends approval of the Preliminary Plat, subject to the conditions described below.</p>

Request:

The applicant is seeking approval of a preliminary plat for the proposed Fremont Commons Subdivision.

Background:

The proposed development is slated to be completed utilizing the principles of Traditional Neighborhood Design (TND) for the residential portion of the project, which is on a tract of land that is approximately 116 acres in size. The proposed development is located at the southeast corner of US Highways 30 and 275. The preliminary plat indicates a mix of housing styles, including row houses, zero-lot line development, and others. The proposed preliminary plat is part of a larger mixed use development that includes retail areas on the north and western edges of the development.

The preliminary plat is the first step in the development process. Before any building permits can be issued, a final plat must be submitted, approved by the Planning Commission and City Council, and filed with the Register of Deeds office.

The proposed development will be serviced by public utilities. However, the applicant has not indicated the size and/or location of utilities on the plat.

The proposed development can be adequately serviced by public safety services within reasonable response times based on previous discussions with city departments.

The property is shown in the comprehensive plan as part of the suburban, auto-urban residential and auto-urban commercial land use classification on the future land use map. Furthermore, this proposed plat coincides with a zoning change request from the current AG Agricultural Urban Reserve zoning district to GC/R-3 districts with a Traditional Neighborhood Overlay District.

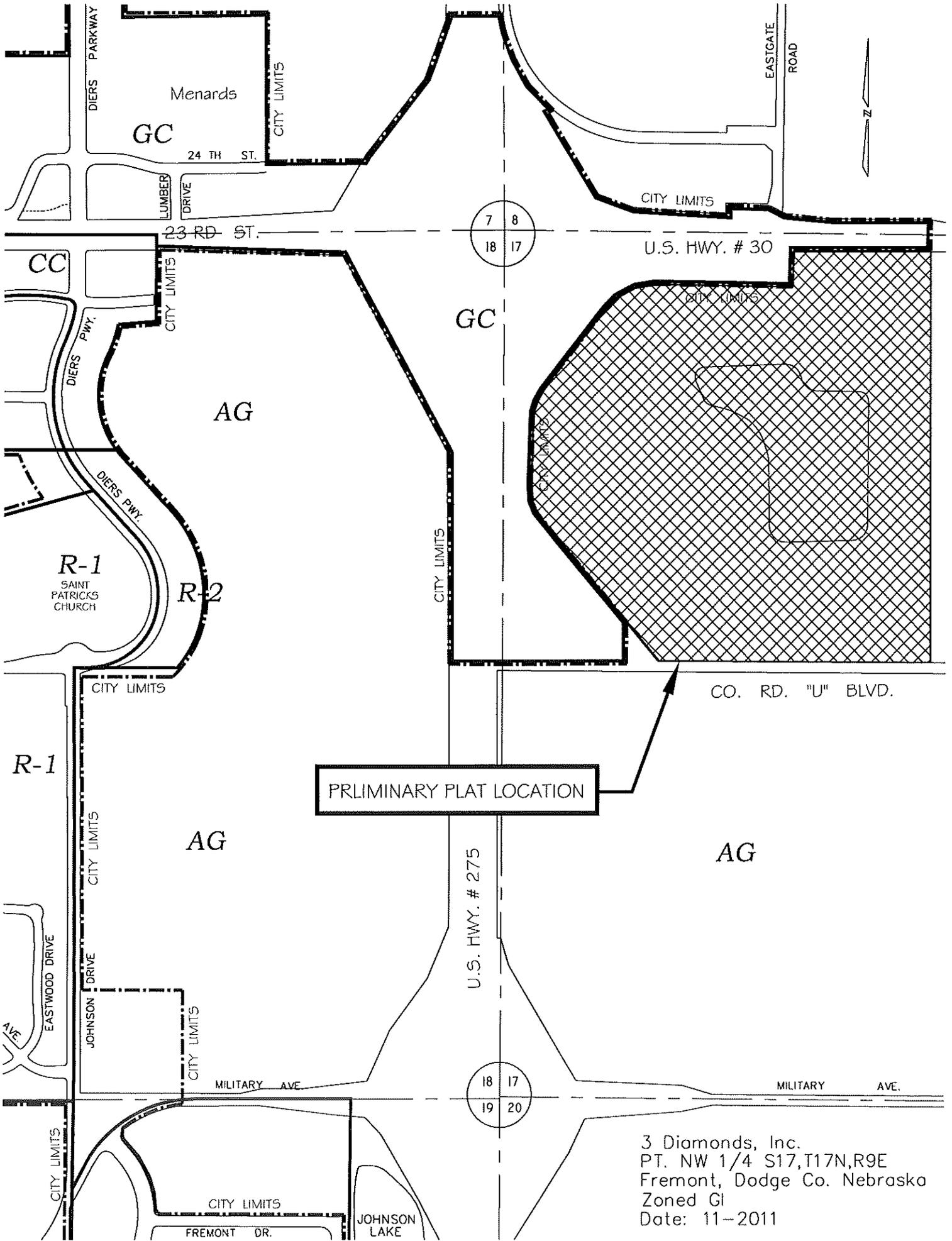
Revisions/Additions:

The following items are requested as part of the approval of the Preliminary Plat:

- 1) Proof of adequate drainage can be shown on the Final Plat.
- 2) Any and all desired covenants will be submitted with the Final Plat,
- 3) Any and all document related to the homeowners association that is mentioned on the Preliminary Plat.
- 4) The Department of Utilities requests that all utilities (type, size, valves, hydrants, etc.) be indicated on the Final Plat.

Findings:

Staff finds the proposed development to be in conformance with the comprehensive plan with regard to proposed land use, economic development potential, and adequate public facilities'. Furthermore, staff feels the proposed development satisfies the intent of the zoning ordinance and subdivision regulations.



PRELIMINARY PLAT LOCATION

3 Diamonds, Inc.
 PT. NW 1/4 S17,T17N,R9E
 Fremont, Dodge Co. Nebraska
 Zoned G1
 Date: 11-2011



U.S. HWY. # 30

U.S. HWY. # 275

CO. RD. "U" BLVD.

Fremont Commons

Design Guidelines

August 2012

Fremont Commons

Design Guidelines

August 31, 2012

Chapter 1: Overview

- 1.1 Introduction
- 1.2 Vision
- 1.3 Neighborhood Design Principals
- 1.4 Definitions

Chapter 2: Neighborhoods

- 2.1 Introduction
- 2.2 Accessory Dwelling Units
- 2.3 Garages
- 2.4 Parks and Open Space
- 2.5 Streets
- 2.6 Alleys

Chapter 3: Residential Standards

- 3.1 Village House
- 3.2 Green Court House
- 3.3 Row Houses
- 3.4 Zero Lot line
- 3.5 Cottage Lot
- 3.6 Materials
- 3.7 Landscape and Streetscape

Chapter 4: Preliminary Plat

Design Guidelines Disclaimer

This Design Guideline document is a preliminary outline for the development standards proposed at **Fremont Commons**. Final zoning restrictions as approved by the City of Fremont may contain additional requirements for parcels within the development. As such, it is anticipated that this set of Design Guidelines will be refined, or amended to incorporate new conditions or requirements. This document is intended to be a fluid and dynamic set of guidelines that will evolve with the Re-Zoning and development process, special circumstances and opportunities that arise. The developer, Fremont Commons LLC, makes no representations or warranties of any kind or character, express or implied with respect to these Design Guidelines.

Chapter 1: Overview



Figure 1.1 A typical street elevation with single family houses of different shapes and sizes.

1.1 Introduction

Fremont is a vibrant city filled with history, charm, commerce, and a strong sense of community. Home to over 28,000 residents and growing, Fremont is a key player within the region, and is poised to become an important contributor in the future growth of the greater Omaha region.

As Fremont grows and evolves, it is important that it not lose the cherished features and amenities that make current residents so proud to call it home. Poorly managed growth brings risk of haphazard, leap-frog and unsustainable development that can adversely impact Fremont for years to come. Aware of these potential pitfalls, the City's comprehensive planning efforts and its *Blueprint for Tomorrow* planning document, is being drafted to identify the best practices for how Fremont can grow in an efficient, sustainable and positive direction.

Fremont Commons has been planned and designed according to these best practices and will provide valuable commercial and residential opportunities for the City's existing and future population. This 127-acre mixed-use development, at the southeast corner of Hwy 30 and Hwy 275, is planned to include large national and small local retailers, a hotel, senior housing, multi-family housing, single-family housing, as well as a twenty-acre community park.

1.2 The Vision

Fremont Commons is founded on the premise of a Traditional Neighborhood Development (TND), a sustainable land ethic, and the cultural values of the region. Through its compact, mixed-use design, tree-lined streets, generous parks and compatible urban design qualities, Fremont Commons embodies the principles of neighborhood and community.

The Fremont Commons neighborhood structure is based on the TND Special Overlay District of the Fremont Zoning Ordinance, and shall incorporate the following characteristics:

- To the extent practical, neighborhoods - the basic building blocks of the community - are designed with a five to ten minute walking distance to the edge to commercial core.
- A mix of residences, shops, workplaces, civic, and recreational uses are included in close proximity to each other.
- Civic uses reinforce the identity of the neighborhood and provide places for purposeful assembly.

- Building frontages in disciplined alignment with appropriate massing and scale define the public spaces.
- Public open spaces in the form of civic plazas, parks, trails, community gardens and neighborhood playgrounds provide places for informal social activity and recreation.
- A variety of thoroughfares serve the needs of pedestrians, bicyclists, and the automobile equitably.

1.3 Neighborhood Design Principles

Fremont Commons has been planned according to the following Design Principles. The Neighborhood Standards identified in the following pages, are intended to promote these Principals:

- Sense of Place
- Diversity
- Connectivity
- Well-defined Public Realm

1.3.1 Sense of Place

Each neighborhood at Fremont Commons will have its own special focus with a network of traditional pedestrian-oriented streets and small parks. Opportunities for neighborly interaction and casual encounters are encouraged whenever possible. Generous porches close to detached sidewalks are located along tree-lined streets. Community gathering places are within a five-minute walk of every home. Carefully placed pocket parks provide the opportunity for easily supervised play for young children or quiet common areas in which adults may socialize. The network of streets, parks and open spaces allow freedom of movement for pedestrians and help to stitch together the community.

1.3.2 Diversity

A primary goal of the Fremont Commons plan is to create interesting, diverse and distinctive neighborhoods through the conscious integration of varying lot types, thoroughfares and architectural styles in a harmonious relationship. A variety of home sizes will be employed to address the needs of different households. The diversity of building types and home sizes not only creates a more appealing neighborhood but also promotes affordable housing opportunities. Diversity of age and income encouraged by the wide variety of housing choices will create a socially vibrant and interesting community.

1.3.3 Connectivity

The system of thoroughfares, trails and pedestrian pathways accessing the neighborhood is predicated on the importance of connectivity and a five-minute walk from doorway to destination. The network of thoroughfares and pathways is designed to promote easy access and mobility for pedestrians as well as motorists. Important connections into and throughout the neighborhood—including school bus stops should respond to the pedestrian scale of the neighborhood.

1.3.4 Well-Defined Public Realm

The Fremont Commons neighborhood places an emphasis on defining the character and quality of the public realm. Private front and rear yards are usually provided, but the focus is on the thoroughfares and open spaces that are distributed throughout the neighborhood. The relationship of the individual buildings to one another and the public space that the buildings create together will make Fremont Commons special. The Green Courts provide alternate pedestrian routes and a green counterpoint to the street network. The majority of the homes provide vehicular access from a secondary street access point, allowing the sidewalks and front yards to be uninterrupted.



Figure 1.3 Large front porches reasonably close to the sidewalk share the outdoor room promoting casual encounters and interaction between neighbors.

1.4 Definitions

ACCESSORY DWELLING UNITS (ADU)

An ADU is a separate dwelling unit that is clearly subordinate to the principal building on a given lot. An ADU may contain its own kitchen and bath facilities and shall be located on the same lot as the principal building. An ADU may have a separate entrance from either the outside of the structure or from within the back building or outbuilding. An ADU does not require the owner to be a full-time occupant at the primary residence on site.

ADDITIONS

A modification to an existing building that expands the square footage of the building or directly modifies any building elevation or roof line.

BLOCK

The aggregate of lots and parcels circumscribed by thoroughfares.

BUILDING

Any permanent, roofed structure built for the shelter and enclosure of persons, animals, materials or property of any kind.

BUILDING COMPONENTS

• PRINCIPAL BUILDING

The principal building is defined as the main structure on the lot containing the primary use.

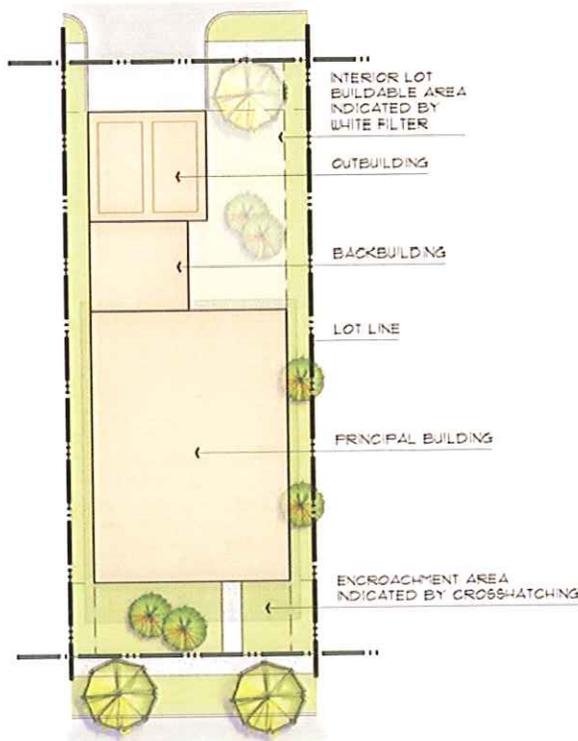


Figure 1.4 Typical Lot Diagram

The outline above illustrates the building envelope defined by the required setbacks. Buildings must be designed to fit within the envelope shown.

• BACK-BUILDING

A back building is a part of the building not exceeding one and a half-stories in height that connects a principal building to an outbuilding.

• OUTBUILDING

An outbuilding is an ancillary building located on the rear of the lot intended for secondary uses such as garage, storage, workshop, studio, guest room or ADU. Outbuildings can be freestanding, detached structures, or attached to the principal building by means of a back-building. When an outbuilding is attached to the principal building by means of a back-building, it is not to be considered a part of the principal building for the purpose of determining setbacks.

BUILDING FOOTPRINT

Building footprint is the total square footage located between and including the foundation walls of all structures on a lot, including garages and carports, but not including open exterior porches located within five feet of adjacent grade.

Roof overhangs, stoops, exterior stairs, and open balconies shall not be included in the building footprint calculation.

BUILDING FRONTAGE

- Building frontage is the total length of the front building wall located within eight feet of the front setback line.
- Porches, fences, or free-standing walls shall not be included in the building frontage. A porch is an open air room appended to the mass of a building with floor and roof, but no walls on at least two sides.
- Building frontage percentage is determined by dividing the building frontage by the total width of the lot at the front setback.
- The front building wall shall be set parallel to a straight front property line and parallel to the chord, if broken or curved.

BUILDING HEIGHT

The total overall height of residential buildings shall be measured from the lowest point of the natural or improved (final) grade, whichever is the more restrictive, to the top most visible structural element (excluding chimneys). One story, two-story and two-and-one half-story residences shall not exceed 35 feet in height.



Figure 1.5 Residential Building Height

The total overall height of mixed-use or retail buildings shall be measured from the improved (final) grade taken along the principal frontage line to the topmost visible structural element. For sloping sites, the measurement shall be taken from the average of the high and low point of measurement along the frontage line. The total overall height shall not exceed 55 feet.

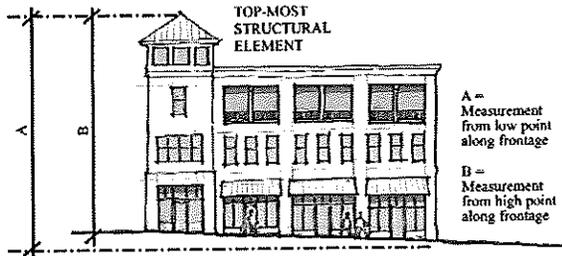


Figure 1.6 Mixed-use building frontage line shown on a gently sloping site. Maximum building height = $(A+B) / 2$.

A story located within the upper-most roof structure shall be considered a half-story when the following conditions are met:

- 60 percent of the exterior walls of the story shall be five feet or less in height.
- No more than 60 percent of the total square footage of the floor directly below is provided on that story.

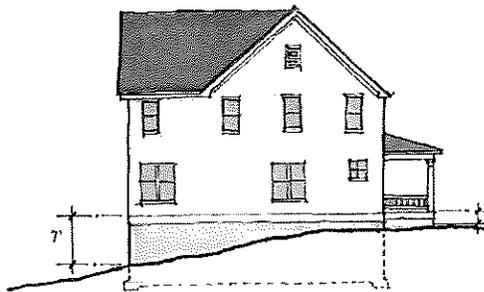


Figure 1.7 Average height from grade to main level finished floor = $(3+7)/2=5.0$. $5.0 < 6$ therefore, basement is not considered a story.

5. A basement, if provided, shall not be considered a story if the measurement from the average finished grade around the perimeter of the building to the finished main floor is less than six feet below the finished main floor level. A basement shall be considered a full-story when the average grade around the perimeter of the building to the finished main floor is greater than six feet.

CARPORT

Carports are defined as attached or detached structures with walls on no more than two sides. A third side may be enclosed by a fence not exceeding six feet in height.

CIVIC

Premises available for but not limited to, not-for profit organizations dedicated to the arts and culture, recreation, education, religion, library, government, social services, public transit, and the like.

COMMON GRADING SOLUTIONS

Contiguous lots shall utilize common grading/drainage solutions unless the City Engineer, requires separate solutions.

DORMER

A projection from a sloping roof that contains a window.

ENCROACHMENT

Hatched areas shown on lot diagrams indicate where building elements may encroach into setbacks. Porches, balconies, bay windows, roof overhangs, cornices, and chimneys are allowed to encroach into the hatched zone indicated on the diagrams.

GARAGE

A garage is a building or part of a building that is specifically designed for the storage of automobiles. Garages can be within an outbuilding, either detached or attached to the principal building by means of a back-building, or attached directly to the principal building. The specific requirements for building placement are included in the lot diagrams for each lot type.

Where garages are in outbuildings, the garages will follow the setbacks listed for outbuildings. The following standards apply to garages:

- Three-car garages are allowed in attached or detached outbuildings only on Village Lot types with a minimum lot width of 65'.
- The maximum enclosed garage sizes are as follows:
 - 2-car garage: 624 SF (24' x 26')
 - 3-car garage: 864 SF (24' x 36')
 - For all other garage sizes, refer to individual lot types
- Exterior stairs, storage sheds and balconies attached to an outbuilding are not considered in the total square footage area for garages.
- A maximum of 50 percent of the lot types that allow three-car garages for a given block may provide a three-car garage. A two-car garage with an additional covered carport (three car spaces) is not considered to be a three-car garage.

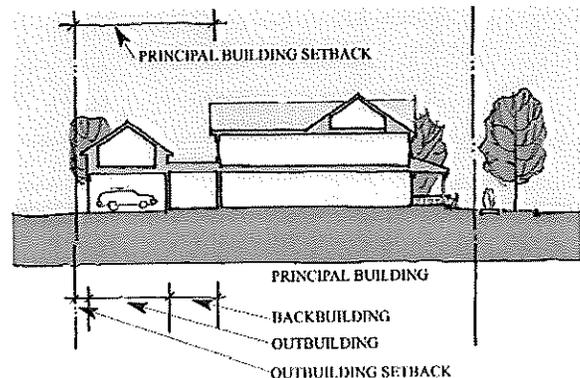


Figure 1.8 A typical Village lot is shown with principal building, back-building, and outbuilding.

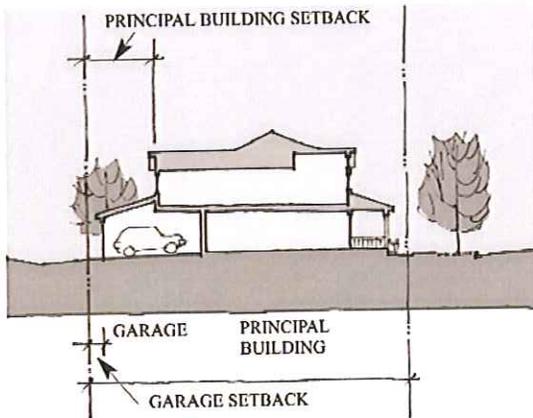


Figure 1.9 A typical Cottage Lot is shown with some 2nd story living space built over the garage to the principal building setback.

INFORMATION KIOSK

A free-standing structure upon which temporary information and/or posters, notices, and announcements are posted, or a free-standing building with one or more open sides from which commercial activities are conducted.

LOT / PARCEL

A parcel of land as established by, and numbered or lettered on, a recorded plat. "Lot" refers to a residential lot. "Parcel" refers to a commercial land designation.

LOT COVERAGE

That area of a parcel which may be occupied by a principal, back-building, or outbuilding and/or accessory structures. Lot coverage shall be considered to be the footprint of the principal, back-building, outbuilding and any accessory buildings measured at grade, including any carport (whether enclosed or not). Overhangs, eaves, decks, and one-story open (roofed) porches shall not be included in lot coverage.

LOT - MAXIMUM LOT COVERAGE

Maximum lot coverage is the percentage determined by dividing the building footprint by the total square footage of the lot. Together with the permitted number of stories, maximum lot coverage controls the intensity of building on an individual lot.

LOT - MINIMUM: WIDTH, DEPTH, AND LOT SIZE

When only minimum lot width and lot depth are given for a specific lot type, minimum lot size equals minimum depth times minimum width. However, when a minimum lot size is listed, the total square footage indicated must be provided.

LOT - MINIMUM WIDTH FOR IRREGULAR LOTS

For irregular lots with uneven sides, the minimum width is to be measured at the principal building's minimum front or rear setback line, whichever is smaller.

MONUMENT SIGN

A permanent free-standing, ground-mounted sign that is detached from the principal building structure. A monument sign shall be set on an architecturally integrated solid base structure that has an aggregate width of at least fifty percent of the width of the sign and uses complementary materials and colors used in the architecture on-site. The total height of the monument sign shall not exceed eight feet above the ground level and shall be designed in context with the adjacent uses on site.

PORCH

A porch is an open air room appended to, or integrated into the mass of a building with floor and roof, but no walls on at least two sides. The porch shall be clearly delineated from the front yard with the use of columns and open railings strongly encouraged. The porch shall be no less than 24 inches and no more than 42 inches above the sidewalk in front of the house.



PUBLIC ART

Any visual or performance work of art, accessible to the public view, on public or private property within the city neighborhood environs including residential, business, or office buildings, apartment and condominium complexes, parks, multiple-use structures, and similar facilities. The work of art may include but not be limited to sculptures, murals, monuments, frescoes, fountains, paintings stained glass or ceramics. Media may include but need not be limited to steel, bronze, wood, plastic, stone and concrete.

A work of art may not contain characteristics of an advertising sign, or identity, or draw attention to a business, profession, or industry, to the type of products sold, manufactured, or assembled, or to the type of services or entertainment offered or available on the premises or in Fremont Commons.

SETBACKS

Setbacks vary according to the specific building type and the desired streetscape character. They are subject to the following rules:

- All building setbacks are measured from the property line.
- When a setback is indicated as a minimum, the exterior building wall must be placed at or behind that line.
- When both a minimum and maximum are given, the exterior building wall may be placed anywhere in between the lines.
- When a setback is indicated as required, the building wall must be placed at that line.
- Side setbacks for principal buildings also apply to back-buildings, but not necessarily to outbuildings.
- Eaves or roof overhangs with up to a three foot projection are allowed within all required setbacks and shall not be counted as part of lot coverage calculations.

SITE WORK

Any activity that requires an excavation or grading permit.

SQUARE FOOTAGE CALCULATION (PARKING PURPOSES ONLY)

Floor area square footage shall be measured to the outside of the framing, not including the exterior cladding material, and shall include all floor areas with a height of five feet or greater. Stairs, elevators, covered porches, exterior arcades, structured parking, and garages shall not be counted in the floor area square footage calculations.

TEMPORARY USE

A use established for a fixed period of time (maximum 180-days) with the intent that such use will terminate automatically upon expiration of the fixed time period unless permission to conduct the use is renewed.

THOROUGHFARE

A public right-of-way designed to accommodate pedestrians, bicyclists and motor vehicles equitably and may include a street, alleyway or public use easement. The streetscape design and building frontage along thoroughfares shall include pedestrian amenities such as seating areas, playgrounds and fields.

VERNACULAR ARCHITECTURE

A style of architecture exemplifying the most common building techniques based on the forms and materials of a particular historical period, region, or group of people.

WALKOUT BASEMENT

A basement that has an exit at grade, although the majority of the basement remains below grade.

Chapter 2: Neighborhoods

2.1 Introduction

Tree-lined streets creating a continuous vegetated canopy are envisioned, with homes oriented to the streets in a way that creates a socially interactive community. Parking garages for residential units will be accommodated primarily along rear alleys or secondary street access points and in auto courts in order to reduce their visual dominance. Homes and units will face the street, mediated by porches or stoops that promote a Traditional Neighborhood approach.



Streetscape Example

While one particular architectural style is not mandated, all buildings within the neighborhoods will be designed to be:

- *Neighborly*, with a strong street-orientation, and with porches and entries that promote interaction and socialization among residents and that reinforce the pedestrian scale and character of the community.
- *Sustainable*, employing strategies to conserve energy and water resources, use healthy long-lasting and low-maintenance building materials, integrate building siting and landscaping, and mitigate light pollution and heat island effect.
- *Compatible*, in scale and character with adjacent structures in the same vicinity.
- *Indigenous*, utilizing to the extent practicable, local materials and regional architectural, and native plant materials.

A series of three building types including: Village Houses, Cottages, and Green Court Houses are envisioned within the single-family portion of Fremont Commons.



Example of Green Court Houses

2.2 Accessory Dwelling Units (ADU)

An ADU is a separate dwelling unit that is clearly subordinate to the principal building on a given lot. An ADU may contain its own kitchen and bath facilities and shall be located on the same lot as the principal building. An ADU may have a separate entrance from either the outside of the structure or from within the back-building or outbuilding. An ADU does not require the owner to be a full-time occupant at the primary residence on site. An ADU is allowed above a 2 or 3-car, attached or detached garage and/or a 2-car garage with an attached carport where an open balcony is provided over the carport below.

Guidelines:

1. ADUs should activate the alley scape.
2. ADUs shall clearly reflect the architectural style of the principal building.
3. The maximum size of an ADU is 624 sf of heated living space.
4. ADUs are required to provide one additional parking space on-site as a carport, third garage space or designated driveway space.
5. ADUs are allowed over two car garage structures only.
6. Carport extension shall be covered by open balcony deck only.
7. Exterior or interior stairways for access to the ADU are allowable.

2.3 Garages

Three types of garage configurations are available for single-family detached houses: detached, semi-detached, attached. Garages may be alley-loaded, or side-loaded, and in two-car, three-car or two-car plus a carport configurations. Lot type diagrams illustrate setbacks and allowable garage configurations. The guidelines and standards apply to both public and private alleyways as necessary.

Guidelines:

1. Garage proportions should demonstrate human scale and not dominate or overwhelm the alley.
2. Garages and/or carports shall be secondary to the principal building.
3. The maximum enclosed garage sizes are as follows:
 - 2-car garage: 624 sf (24' x 26')
 - 3-car garage: 864 sf (24' x 36')
4. Garage detailing shall have visual interest using similar materials to the principal and back buildings.
5. Garages and/or carports shall match the principal building and back-building in architectural style.
6. Three-car garages are discouraged, however, may be allowed on a limited basis, with a 20 percent maximum per block.
7. Two-car garages with covered (or non-covered) carports are allowed



2.4 Parks and Open Space

Parks and public open spaces provide an opportunity for design continuity and integration between the built urban form and the natural and 'constructed' landscapes within the Fremont Commons neighborhood.



Parks and public spaces include neighborhood and community parks, open spaces, thoroughfares, pocket parks, civic plazas, green courts, natural buffers, the lake, and wetland areas.



Park lands and civic spaces should be appropriately programmed and designed to have well defined areas for activity that are among passive recreational and natural areas.



2.5 Streets

Streets and thoroughfares, representing the entry and passageways throughout the community, should be designed and maintained to provide a pleasant and welcoming aesthetic. As public spaces, the street landscape serves to accommodate a variety of people and activities while helping to educate and promote an awareness of the sustainable design practices adopted as a part of the neighborhood. A variety of thoroughfares serve the needs of pedestrians, bicyclists, and the automobile equitably. Generous sidewalks and bicycle paths connect the neighborhoods to the commercial districts of Fremont Commons.



2.6 Alleys

Alleys shall facilitate access to on-site parking, garages, and accommodate use by service, delivery and emergency vehicles. Alleys shall provide a clear fire lane access throughout the development.

Guidelines:

1. There shall be a minimum of 30' of distance between building faces on opposite sides of an alley on all residential neighborhoods.
2. Public alleys shall be platted with a 20' right-of-way with a minimum pavement width of 16'. Twenty-two (22') feet pavement width shall be maintained within 20' of street intersections and alley intersections.
3. At Green Court conditions, when there is no street in front of the lot, and when less than 24' of pavement is used in an alley, an edge treatment capable of supporting fire equipment (80,000 lbs) must be used for a total width of 20'.
4. Underground utilities located in alleys shall be placed to share both the 5' rear yard setback area (inside the private property line) and may be placed on both sides of the alley right-of-way.



Chapter 3: Residential Standards

3.1 Village House – Alley Loaded Lot

The Village Lot is the basic single-family building block of the neighborhood. Village Lots integrate with Green Court Lots and Cottage Lot types to create a varied pedestrian-friendly streetscape. Village and Green Court Lots are typically alley loaded conditions, and Cottage lot types are typically front loaded lots. Garages may be detached or attached by means of a one-story back-building. Lot widths can vary to efficiently allow for different models and styles of homes.



The Village Houses are single-family detached lots, generally ranging in size from 50' x 120' to 60' x 100' promoting a diversity of housing opportunities. The following guidelines apply to all Village Lots:

Auto access: Village Houses will primarily be served by rear alleys with garages set a minimum of 20' from the property line.

Variation:

- Each block face should contain at least four different floor plan models with no more than two of the same building elevation. A separation of at least four lots should be maintained for any model with similar elevations, colors or materials.
- A mix of single, one-and one-half, and two story homes should be along a block face.
- A mix of materials, colors and detailing should be employed.

Porches: All Village houses will have ground level front or corner porches with a minimum area of 90 square feet, and a minimum depth of seven feet. The porch shall be no less than 24 inches, and no more than 42 inches above the sidewalk in front of the house.

Corner Lots: Buildings on corner lots should be sited and specially designed so that they present attractive elevations to both streets. Building and landscape elements, house massing, wrap-around porches, façade composition and other design strategies should be employed. Where a garage presents its side elevation to the street, it should be specially designed as an extension of the of the principal building elevation.

Roof Forms: Village Houses should have sloping roofs, employing gables, hips and dormers. The roof pitch of the principal building should generally utilize slopes of 6:12 up to 9:12. Greater slopes up to 12:12 may be appropriate where

living space is incorporated into the roof structure. Mansard and false roofs are not permitted.

Rooftop equipment (satellite dishes, vent stacks, solar panels) shall be architecturally integrated within the design and not visible from the street or other public areas.

Lot Size	Width	50' min.
	Depth	100' min.
Lot Coverage (excludes porches)		65% max.
Setbacks		
Principal building	at Street	15' min.
	at Street	20' max.
	at Side	5' min.
Principal building	at Rear	40' min.
Outbuilding	at Alley	20' min.
Outbuilding	at Street	10' min.
Outbuilding	at Side	5' min.
Back -building	at Street	10' min.
Back-building	at Side	5' min.
Building Frontage within 8' of Setback		50% min.
Encroachments	at Street	8' max.
	at Side	2' max.
	at Rear	2' max.
Height		
Principal Building	2 1/2 stories	35' max.
Back-building	1 story	20' max.
Outbuilding	1 1/2 stories	25' max.
	(2 stories are allowed with detached garages)	
Accessory Dwelling Unit (ADU)		Allowed

Notes:

1. Maximum allowed garage size is 864 sf (3-car garage), allowed only on minimum lot width of 70'. (Back-building may be used for 3rd car tandem parking space.)
2. Garages can be attached only through a maximum 18' wide back-building containing 100 sf min.

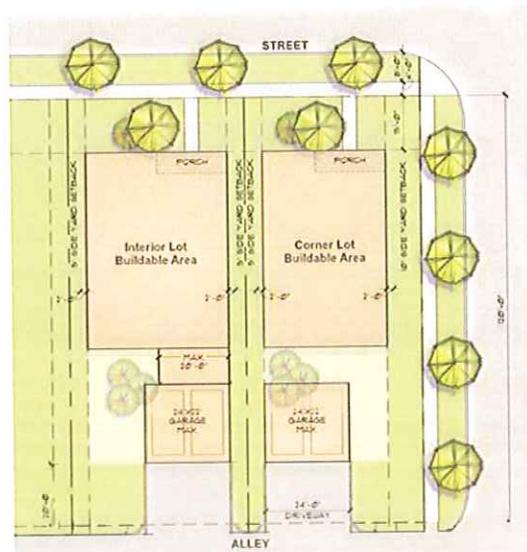


Figure 2.1 Typical Village Lot

3.2 Green Court House

Green Courts are groupings of four or more Village Houses around a common green. The grouping of Green Court Lots provides a memorable and gracious counterpoint to the more typical streetscapes with rows of single-family houses. The grouping creates a mini-neighborhood promoting a sense of community. They provide a safe place for small children to play, and accommodate public pedestrian linkages through the neighborhood, connecting thoroughfares together without adding pavement. Green Court lots typically employ shorter lot depths and work well with garages attached to the rear or side of the residence. The grouping is a composite of lot types including Village and Cottage Lots. The 45' Village Lot is typically at the end of a Green Court with the Green Court Lots located in between.



The design of the Green Court Houses will be governed by the guidelines for Village Lots with the following additional provisions:

Relationship to Street: Porches may encroach up to five feet into this setback area. The porch shall be no less than 24 inches, and no more than 42 inches above the sidewalk in front of the house. The design of the Court should be open and welcoming to the street with any fencing and landscaping no higher than 36 inches. The side elevation of a Green Court home (including its attached garage) along a public street should be specially designed with the same level of architectural detail as a front elevation. (See Corner Lots above in section 3.1).

Green Court: The width of the “green court”, independent of the individual lots, will be no less than 25 feet. It should include a walkway of at least four feet in width providing a direct path of travel to each residential lot along the court.

Front Yard Facing Green: Green Court houses will be set back from the green by 12 feet; porches may encroach up to five feet into this setback area.

Auto Access: All Green Court houses must be served by rear or side streets. The street should be substantially screened from the green court through the placement of buildings and landscaping.

Variation: Within a Green Court complex, there should be no more than two units with the same building elevation. The massing of individual buildings should be varied to create visual interest, and to reinforce the spatial composition of the complex.

Lot Size	Width	45' min.
	Depth	100' min.
Lot Coverage (excluding porches)		65% max.
Setbacks		
Principal building	at Street	32' min.
Principal building (front)	at Green Court	12' min.
Principal building	at Side	5' min.
Principal building	at Rear	32' min.
Outbuilding	at Alley	20' min.
Outbuilding	at Side	5' min.
Outbuilding	at Street	10' min.
Back -building	at Street	10' min.
Back-building	at Side	5' min.
Building Frontage within 8' of Setback		50% min.
Encroachments		
	at Street	8' max.
	at Green Court	2' max.
	at Side	2' max.
Height		
Principal Building	2 1/2 stories max.	35' max.
Back-building	1 story max.	20' max.
Outbuilding	1 1/2 stories	25' max.
ADU	Allowed	

Notes:

1. Maximum allowed garage size is 528 sf (plus potential carport) for all lot types
2. Garage setback from a street between 8' and 18' are not allowed.



Figure 2.2 Green Court

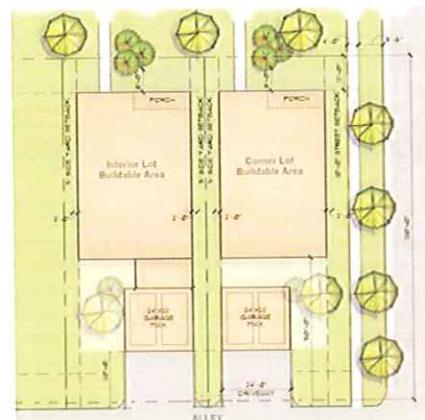


Figure 2.3 Green Court 50' Typical Lots

3.3 Row House

The Row House at Fremont Commons offers a single family attached residence that can be either a rental or for-sale product. The concept provides for a 25' x 95' lot size, with a courtyard between the garage and the living unit. All lots are alley loaded, with the front door of each unit facing the public street or landscape area.

The Row House lot utilizes a deep lot and a detached garage to create a well-defined backyard or courtyard between the principal building and garage. Lot width can be increased to 30 feet where the Row House abuts a thoroughfare or to create a break between buildings.



The Row Houses are located as a transitional use between the Commercial and Residential Districts of Fremont Commons. As such, the Row House is conceived as a more contemporary building form which will relate to the higher density lifestyle of this building type. The Row House design will be governed by the following standards:

Auto access: Row House Lots will be served by rear alleys with garages set a minimum of 20' from the alley. Setbacks from the alley between 5' to 18' are not allowed.

Porches / Stoops: All Row Houses will have ground level front or corner porches with a minimum area of 50 square feet, and a minimum depth of six feet. The porch shall be no less than 24 inches, and no more than 42 inches above the sidewalk in front of the house.

Building Massing: The Row House is envisioned to be two stories in height, not to exceed 30 feet. The buildings should employ changes in plane of the front façade, sloped roofs, and balconies at the second floor to help achieve variety in the building massing. Row House buildings should be a maximum of 150 feet in length, with a 10 foot wide passage way connecting the street to the alley.

Courtyards: Each Row House should incorporate a private courtyard of no less than 400 square feet, located between the garage and the main living unit.

Front Yard Setbacks: The front wall of Row Houses will be set back from the property line a minimum of 10 feet by no more than 20'. Porches, stoops, chimneys and roof overhangs may encroach into the setback area by up to two feet. Corner lots will require both street frontages to be considered a front yard.

Lot Size	Width	25' min.
	Depth <td>95' min.</td>	95' min.
Lot Coverage	(excluding porches)	75% max.
Setbacks		
Principal building	at Street	10' min.
	at Street	20' max.
	at Corner Street	15' min.
	at Side	0'
Principal building	at Rear	48' min.
Outbuilding	at Alley	20' min.
Outbuilding	at Street	15' min.
Back -building	at Street	15' min.
Building Frontage within 8' of Setback Encroachments	at Street	50% min. 5' max.
Height		
Principal Building	2 stories	30' max.
Back-building	1 story	20' max.
Outbuilding	1 1/2 stories	25' max.
ADU	Not Allowed	

Notes:

1. Maximum allowed garage size is 480 sf (2-car garage) per unit.
2. Garages on the end lots can be attached to the principal building only by a back-building with 12' maximum width and 160 sf minimum.
3. Garage setbacks from an alley between 8' and 18' are not allowed.

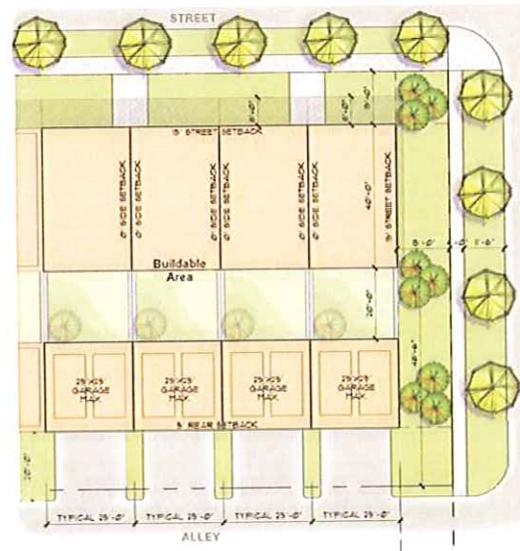


Figure 2.4 Typical Row House



3.4 Zero Lot Line

In a zero lot line the house is built on one side at the property lot line in order to create more usable space. For this reason the lot widths can be smaller than a typical lot in this case 45' wide. The grouping creates a sense of community where neighbors know each other and are connected through a common thoroughfare and common amenity areas. These grouped housing units have a common community park or open space no more than a ¼ mile away from their house. These lots can be attached as in a townhome or detached and be a single story or multistory. These lots are either located in areas where they can include a green court or areas on the site where they have front and rear access points.



Relationship to Street:

Porches are encouraged and should not be less than 24 inches, and no more than 42 inches above the sidewalk in front of the house. In areas where these are in a Green Court condition these units should be open and welcoming to the street with any fencing and landscaping no higher than 36 inches. The side elevation of a Green Court home (including its attached garage) along a public street should be specially designed with the same level of architectural detail as a front elevation. (See Corner Lots above in section 3.1).

Green Court: In the event that Zero Lot line houses face a common Green Court, the width of the "green court", independent of the individual lots, will be no less than 25 feet. It should include a walkway of at least six feet in width providing a direct path of travel to each residential lot along the court.

Front Yard Facing Green: Green Court houses will be set back from the green by 12 feet; porches can encroach up to five feet into this setback area.

Auto Access: These will be served by rear and a side street. The street should be substantially screened from the green court through the placement of buildings and landscaping. The main street will be used for vehicle access and the secondary or rear street will be used as for pedestrian access. Sidewalks are located on both streets for better connectivity with the rest of the development.

Variation: Within a Green Court complex, there should be no more than two units with the same building elevation. The massing of individual buildings should be varied to create visual interest, and to reinforce the spatial composition of the complex.

Lot Size	Width	45' min.
	Depth	100' min.
Lot Coverage (excluding porches)		65% max.
Setbacks		
Principal building	at Street	32' min.
Principal building (front)	at Green Court	12' min.
Principal building	at Side	5' min.
Principal building	at Rear	40' min.
Outbuilding	at Alley	20' min.
Outbuilding	at Side	5' min.
Outbuilding	at Street	10' min.
Back -building	at Street	10' min.
Back-building	at Side	5' min.
Building Frontage within 8' of Setback		
Encroachments	at Street	50% min.
	at Green Court	8' max.
	at Side	6' max.
		2' max.
Height		
Principal Building	2 1/2 stories max.	35' max.
Back-building	1 story max.	20' max.
Outbuilding	1 1/2 stories	25' max.

Notes:

1. Maximum allowed garage size is 528 sf
2. Garage setback from street between 8' and 18' are not allowed.
3. Corner Lots minimum 45' width.



Figure 2.5 Zero Lot line 45' Lots

3.5 Cottage Lot - Front-Loaded Lot

The Front-Loaded lot is located where use of a secondary street access point for the lot is difficult or inefficient. These lots allow for the driveway and garage to be accommodated, while still providing adequate thoroughfare frontage for the principal building. Garages are either detached and placed behind the main house with side drive access or attached to the principal building with a minimum of 15' offset to the garage.

50' Lot Size	Width	50' min.
	Depth	120' min.
Lot Coverage (excludes porch & other exterior covered areas)		40% max.
Setbacks		
Principal building	at Street	15' min.
Garage Setback from Principal Building Face	at Side	5' min.
	at Rear	40' min.
(Corner lot condition)	at Rear	34' min.
Outbuilding	at Rear	10' min.
Outbuilding	at Side	5' min.
Back -building	at Street	10' min.
Building Frontage within 8' of Setback		
Encroachments	at Street	35% min.
	at Side	8' max.
	at Rear	2' max.
Height		
Principal Building	2 stories	30' max.
Back-building	1 story	20' max.
Outbuilding	1 1/2 stories	25' max.
ADU	Allowed (only behind principal building)	

Notes:

1. Total width of the garage doors facing the street shall not exceed 18'.
2. When attached, the garage needs to have a 15' min. setback from the front wall of the house.
3. When garage front wall is placed in front of the principal building's front wall, garage width (parallel to the front property line) shall not exceed 30% of the building frontage.

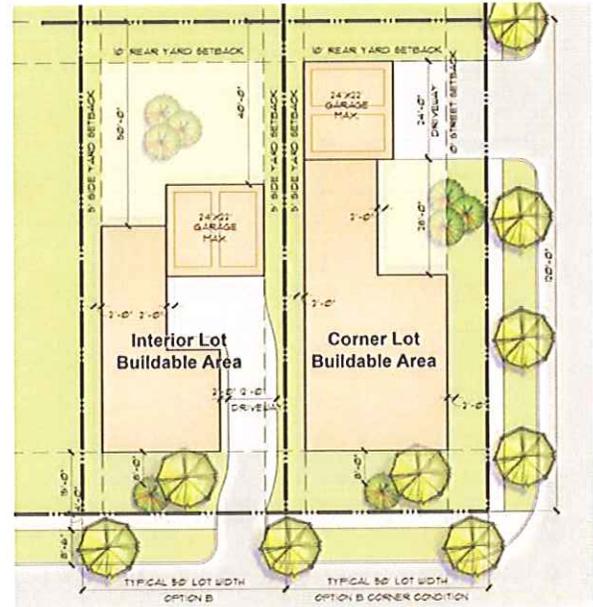


Figure 2.6 Typical Front Loaded Lot

60' and 70' Lot Size	Width	60' min.
	Depth	100' min.
Lot Coverage (excluding porches)		40% max.
Setbacks		
Principal building	at Street	15' min.
Garage Setback from Principal Building Face	at Side	15' min.
	at Rear	5' min.
(Corner lot condition)	at Rear	40' min.
Outbuilding	at Rear	34' min.
Outbuilding	at Side	10' min.
Back -building	at Street	5' min.
	at Street	10' min.
Building Frontage within 8' of Setback		
Encroachments	at Street	50% min.
	at Side	8' max.
	at Rear	2' max.
Height		
Principal Building	2 stories	30' max.
Back-building	1 story	20' max.
Outbuilding	1 1/2 stories	25' max.
ADU	Allowed (only behind principal building)	

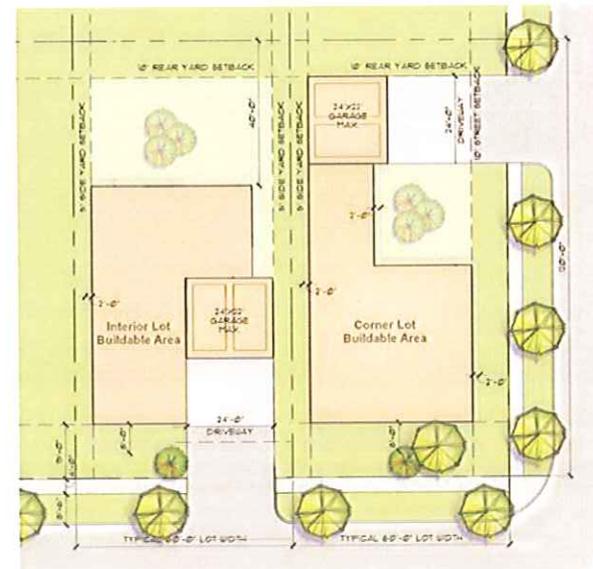


Figure 2.7 Typical Front Loaded Lot

3.6 Materials

Neighborhood buildings should be designed in the spirit of regional Mid-west architecture, in a traditional or contemporary idiom. Hip and gabled roofs with dormers, projecting eaves, and the use of porches and stoops at the entry points is encouraged. Preference should be given to materials and products that enhance building energy performance and that are manufactured with raw materials that are non-toxic, low-emitting, recycled, recyclable, and or regionally sourced and manufactured.

Building Materials: A simple and harmonious application of materials is encouraged in keeping with the form and style of the building. An excessive number of materials is discouraged; material changes should occur when there is a change in volume and or plane. Materials should wrap around to the sides of the building to promote three dimensional design. Acceptable cladding materials include: smooth finish clay brick in Common, English or Flemish bond patterns; painted brick; smooth finished stucco; smooth horizontal bevel or lap fiber-cement siding in four to eight inch wide planks, with miter cut corners or with 5/4 by 6 inch corner boards. Vinyl siding is not permitted in any location.

Windows: In keeping with the spirit of traditional regional architecture, the following characteristics should be followed:

- Windows should be vertically proportioned with a minimum vertical to horizontal proportion of 1.6 : 1.
- Double-hung, single-hung, casement and awning windows are permitted. Sliding horizontal windows are not permitted.
- Wood, vinyl or metal clad, or aluminum windows with traditional profiles and double-glazed are permitted.
- Shutters should be proportioned and sized so that they could enclose the window if operable.
- All windows should include surrounding trim appropriate to the materials, style and proportion of the home.

Exposed Foundations: Exposed building foundations should not exceed 24 inches above grade. When more than 12 inches of foundation is visible, cover walls should be provided with masonry, painted or finished with stucco or board formed cast texture.

Fencing and Walls: Hedges or low fences of wood or metal up to 36 inches in height are permitted along a fronting property line. Privacy fences up to a height of 60 inches are permitted on side yards and rear yards provided that the side yard privacy fence is set back at least 10 feet from the front, and three feet from the rear building façade. Fences should be coordinated with the design of the building in terms of color, scale, and detailing. Along alleys, fences are encouraged to permit some views into the rear yard and to have gates that provide pedestrian access as applicable.

3.7 Landscape and Street Scap

Tree-lined streets creating a continuous vegetated canopy are envisioned throughout the residential neighborhoods, commercial district, thoroughfares and park areas. The following key elements will be required of all developers building homes in **Fremont Commons**:

Street Trees: All residential lot types (Village, Green Court and Cottage Lots) will have a minimum of one Street Tree, and one shade or ornamental tree in the front yard of each lot. Street trees will generally be evenly spaced along a block face, and will be located between the sidewalk and the curb.

Rear Yard Trees: All Village and Cottage Lots will have a minimum of one shade tree in the rear yard of each lot.

Ground Cover: All exposed and unpaved natural soil shall be planted with turf and / or ground cover.

Sidewalks: A continuous six foot wide sidewalk shall be provided on both sides of every street.



Alleys: Planting pockets shall be provided at each lot for shrubs and vines that can grow adjacent to the fenced areas along the alley.



Street and Bioswale Cross Sections: The residential streetscape environment will feature the use of bioswales in order to provide a more naturalistic approach to storm water management (figures 3.1 and 3.2). In the typical cross section, the street will be gently crowned at a grade of no more than 2% to direct storm water to the swales. The water will enter the swales through a "broken curb" condition. The swales will incorporate a gravel underdrain or french drain in order to encourage the infiltration of the storm water back into the soil. The swales may incorporate either a turfgrass type

ground cover planting (Turf Type Tall Fescue) or, alternatively, a native grass seed mix ground cover planting consisting of Buffalograss, Blue Grama, Sideoats Grama and Little Bluestem (Prairie 3 Plus Mix, Stock Seed Farms, Murdock, Nebraska). Street trees will also occur within the bioswale cross section, and be located on gently raised earthen mounds. These serve as checkdams and also elevate the root ball of the trees out of the bottom of the swale. A consistent species of tree shall be used on a street in order to create a more uniform tree canopy and streetscape appearance. Street trees are selected for their size, canopy, hardiness, durability and ability to withstand both moist and dry soil conditions. Street trees will be spaced as uniformly as possible, at a spacing of approximately 60 feet on center. The concept is to provide a street tree at each individual residential lot.

Two alternative Bioswale / Sidewalk conditions may occur within Fremont Commons. Figure 3.1 illustrates a typical 55' ROW Street Section, with 6' sidewalks on either side of the street. Figure 3.2 illustrates a 55' ROW condition where one of the sidewalks is expanded to an 8' wide "trail" section which will connect with the path and trail system through the park area, and a 4' wide sidewalk on the opposite side of the street.

Driveway Bioswale Crossings: The driveway crossing detail has been developed in order to allow access to front-loaded lots along residential streets having bioswales. This detail maintains the advantages of the bioswale approach to stormwater management. The reinforced concrete structure spans the swale and provides a large opening in order to permit the flow of water through the cross section.

Street Lighting: As indicated in the typical cross sections, street lights will be located with the right-of-way, at the "outside" edge of the six foot wide sidewalk. Street lighting will typically occur at intersections, rather than at mid-block locations.

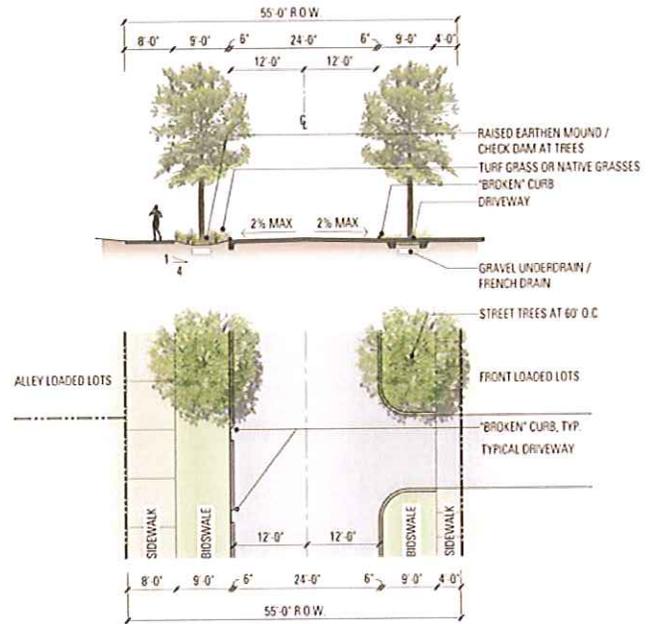


Figure 3.2 Typical Street Section 55' ROW, with 8' Trail, and 4' Sidewalk

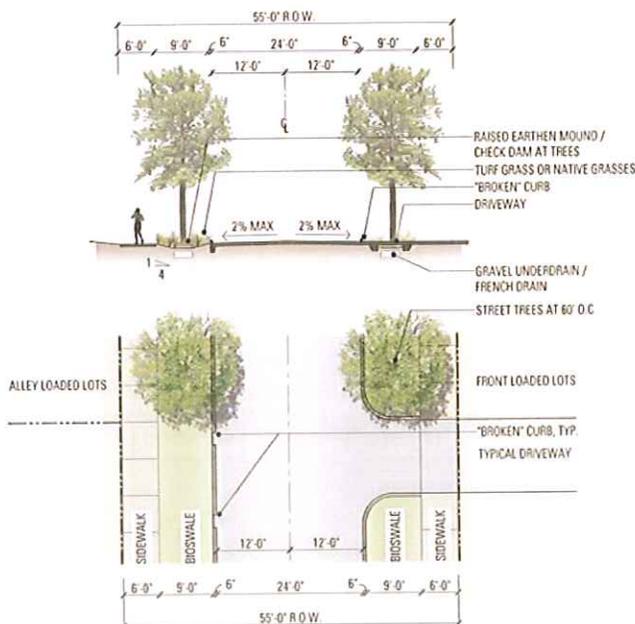


Figure 3.1 Typical Street Section 55' ROW, with 6' Sidewalks

LEGAL NOTICE

The Fremont Planning Commission will consider the Preliminary Plat of Fremont Commons, consisting of a tract of land located in the Northwest Quarter of Section 17, Township 17 North, Range 9 East, Dodge County, Nebraska, at their regular meeting on September 17, 2012 at 5:00 p.m. in the City Council Chambers at 400 East Military Avenue.

Publish: September 7, 2012

SEE NEXT PAGE

LEGAL NOTICE

The Fremont City Council will consider the Preliminary Plat of Fremont Commons, consisting of a tract of land located in the Northwest Quarter of Section 17, Township 17 North, Range 9 East, Dodge County, Nebraska, at their regular meeting on September 25, 2012 at 5:00 p.m. in the City Council Chambers at 400 East Military Avenue.

Publish: September 15, 2012

STAFF REPORT

TO: Planning Commission

FROM: Rian Harkins, Planning Director

DATE: 10 September 2012

SUBJECT: Request by Fremont Commons to rezone the NW ¼ of Section 17, Township 17 North, Range 9 east of the 6th P.M., Dodge County, Nebraska from AG Agricultural/Urban Reserve To GC General Commercial & R-3 Mixed Density residential with a Traditional Neighborhood Development Overlay District (Fremont Commons, LLC)

Recommendation: Staff recommends approval.

Request:

The applicant is seeking approval of a zoning change from AG Agricultural/Urban Reserve To GC General Commercial & R-3 Mixed Density residential with a Traditional Neighborhood Development Overlay District.

Background:

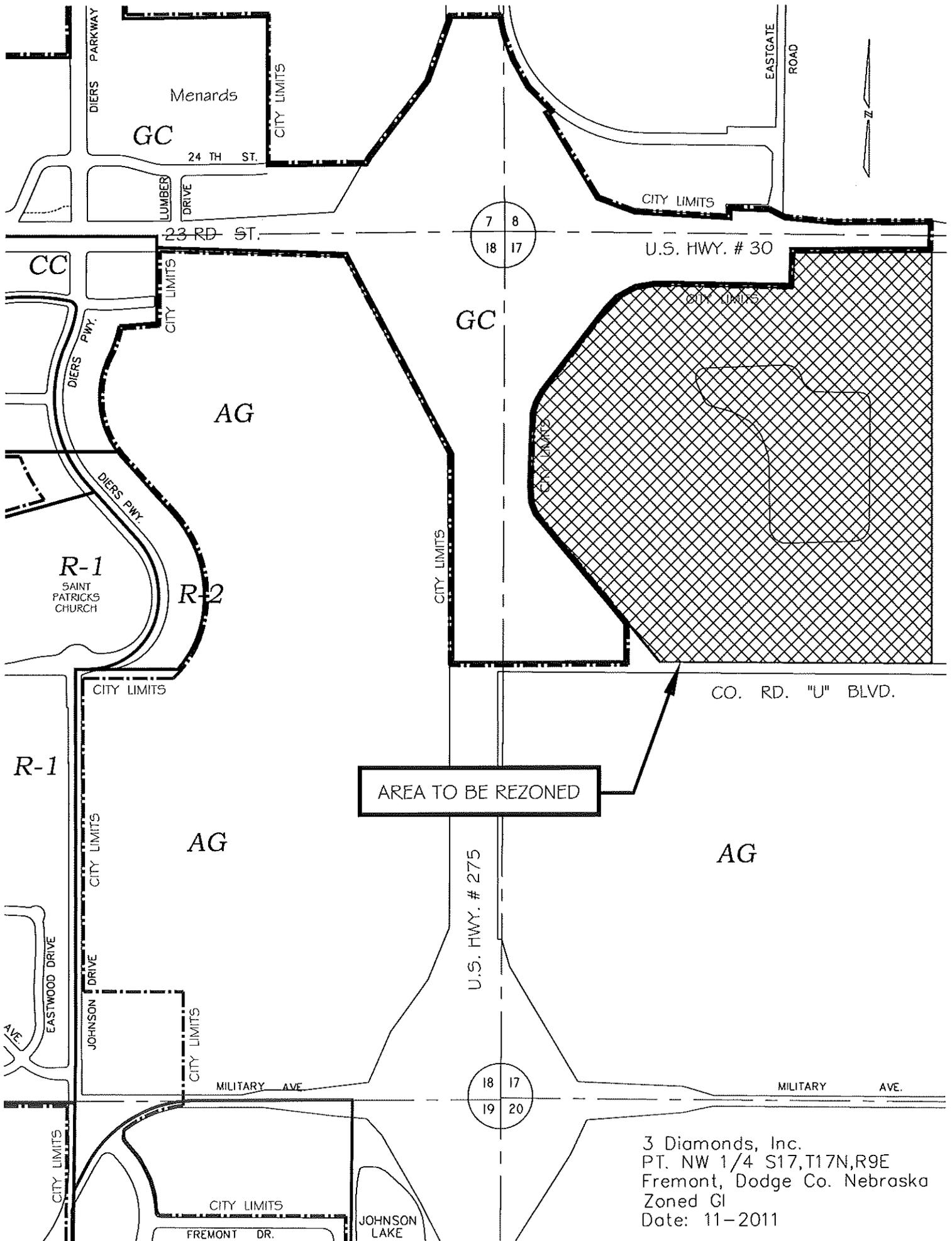
The proposed development is slated to be completed utilizing the principles of Traditional Neighborhood Design (TND) for the residential portion of the project, which is on a tract of land that is approximately 116 acres in size. The proposed development is located at the southeast corner of US Highways 30 and 275. The development plan and design guidelines, which are included in your packet for review, indicates a mix of housing styles, including row houses, zero-lot line development, and others. The proposed preliminary plat is part of a larger mixed use development that includes retail areas on the north and western edges of the development.

The proposed development contains a large park that abuts residential development on three sides, with the commercial development located to the north of the park.

The establishment of the TND overlay allows for the standards of the underlying zoning to be flexed in order to utilize the TND concepts.

Findings:

Staff finds the proposed development to be in conformance with the comprehensive plan with regard to proposed land use, economic development potential, and adequate public facilities'. Furthermore, staff feels the proposed development satisfies the intent of the zoning ordinance and subdivision regulations.



AREA TO BE REZONED

3 Diamonds, Inc.
 PT. NW 1/4 S17,T17N,R9E
 Fremont, Dodge Co. Nebraska
 Zoned G1
 Date: 11-2011



U.S. HWY. # 30

U.S. HWY. # 275

CO. RD. "U" BLVD.

STAFF REPORT

TO: Planning Commission

FROM: Rian Harkins, Planning Director

DATE: 10 September 2012

SUBJECT: Request of Deer Pointe Corporation to approve the Final Plat of Deer Pointe Second Addition to the City of Fremont (Lot 6, Block 2, Deer Pointe Addition and part of the South ½ of the NW ¼ of Section 18, Township 17 North, Range 9 East of the 6th P/M/, Dodge County, Nebraska) (Deer Pointe Corp.)

Recommendation: Staff recommends approval.

Request:

The applicant requests the property listed above be approved as the final plat of Deer Pointe Second Addition.

Background:

The property is located on the east side of the city, adjacent to the corporate boundary, along Deer Pointe Drive. The area surrounding the property is a combination of agricultural and residential land uses, with the corresponding zoning districts being AG Agricultural/Urban Reserve and R-1 Single Family Residential.

The applicant has had a sketch plat reviewed by the planning department in cooperation with other city departments. The property was approved for rezoning from AG Agricultural/Urban reserve to R-1 Single Family Residential last month.

Findings:

The proposed final plat will allow the property to be developed for additional residential development at an urban scale. The final plat request does meet the intent of the R-1 Single Family Residential zoning district, is in the general growth pattern of the community, and is in conformance with the Comprehensive Plan, Blueprint for Tomorrow.



STAFF REPORT

TO: Planning Commission

FROM: Rian Harkins, Planning Director

DATE: 10 September 2012

SUBJECT: Study Session – Board of Adjustment & Findings of Fact

Recommendation: None, this is merely a study session item.

Background:

As part of an effort to provide additional training opportunities, Mr. David Ptak will be speaking on the two items noted above at the Planning Commission meeting.